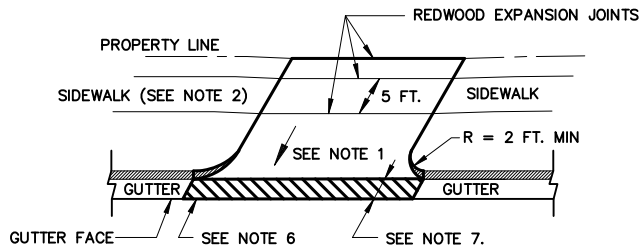
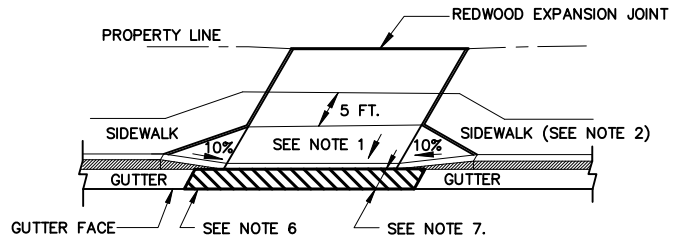


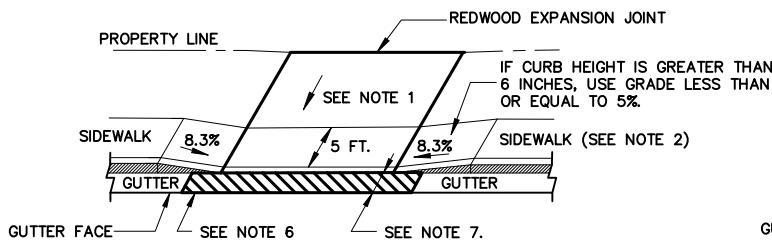
DRIVEWAY APPROACH WITH SETBACK SIDEWALK



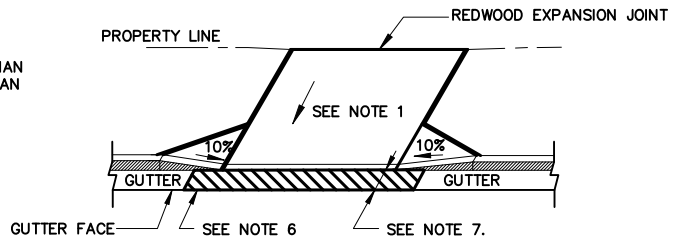
DRIVEWAY APPROACH WITH OFFSET SIDEWALK



DRIVEWAY APPROACH WITH RAMP SIDEWALK



DRIVEWAY APPROACH WITHOUT SIDEWALK



NOTES:

1. DRIVEWAY APPROACH: MAX. SLOPE 5%, MIN. SLOPE 2%.
2. SIDEWALK: MAX 2% MAX CROSS SLOPE AND 5% LONGITUDINAL SLOPE.
3. PAVING HEADER IS NOT REQUIRED IF STREET HAS A CURB.
4. SAW CUTTING OF STREET IS NOT REQUIRED IF STREET DOES NOT HAVE CURB.
5. IF ASPHALT SHOULDER EXISTS, REMOVE SHOULDER & TIE CONCRETE DRIVEWAY INTO CONCRETE STREET & REPAIR SHOULDER AS NECESSARY. (EXAMPLE: GARTH RD).
6. FOR STANDING CURBS AND/OR DIRECTED BY ENGINEER, SAW CUT GUTTER LIP, REMOVE EXISTING CONCRETE, AND REPLACE AS PART OF NEW DRIVEWAY APPROACH (SEE SHEET DWY-07).
7. 2 FT. FROM BACK OF CURB OR EDGE OF PAVEMENT.

SEAL:



CITY OF BAYTOWN
DEPARTMENT OF ENGINEERING

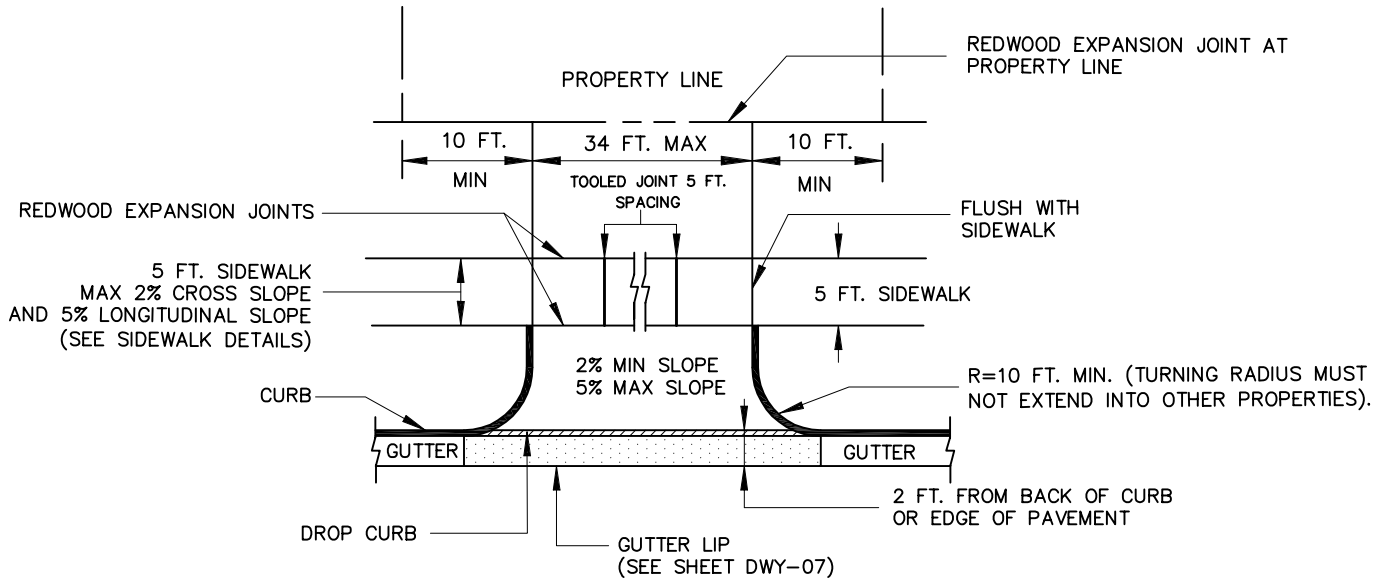
GENERAL COMMERCIAL & RESIDENTIAL
DRIVEWAY APPROACH LAYOUTS (CURB & GUTTER)

REVISION	DATE
ADD "DRIVEWAY APPROACH" FORMAT CHANGE	01/26/17

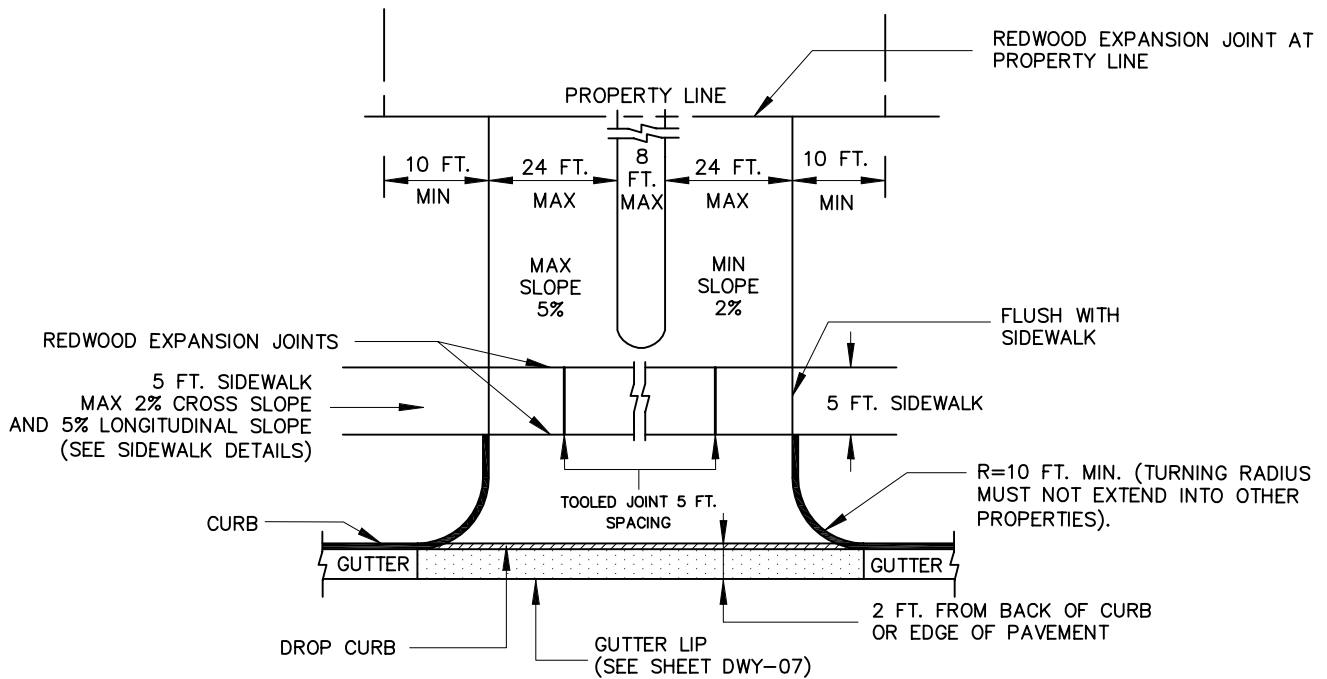
EFFECTIVE DATE: JANUARY 18, 2017

DRAWN BY: T.L.	APPROVED BY: J.A.P.
CHECKED BY: A.S.	JOSE PASTRANA DIRECTOR OF ENGINEERING
SCALE: N.T.S.	SHEET NO: DWY-01

COMMERCIAL DRIVEWAY APPROACH WITHOUT MEDIAN



COMMERCIAL DRIVEWAY APPROACH WITH MEDIAN



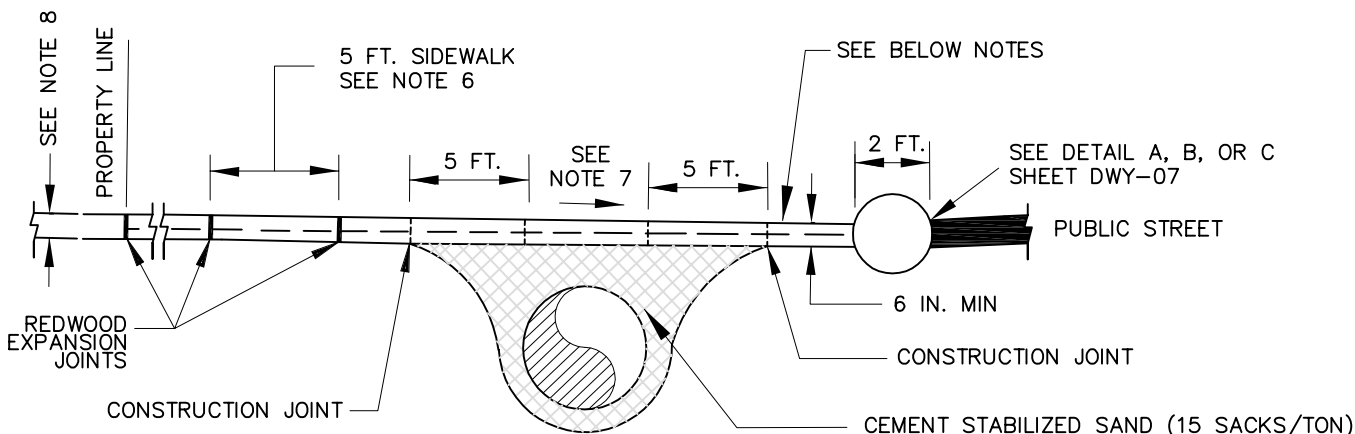
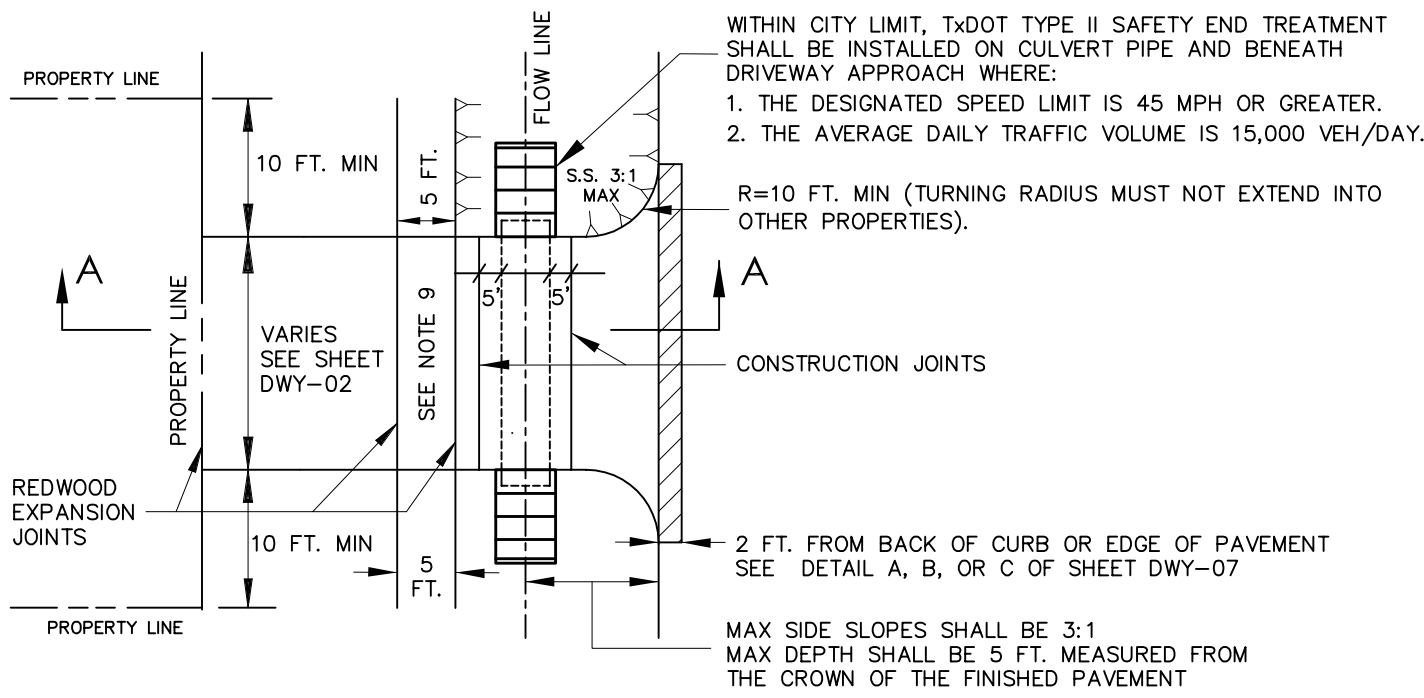
SEAL:



CITY OF BAYTOWN
DEPARTMENT OF ENGINEERING

COMMERCIAL DRIVEWAY APPROACH

REVISION	DATE	DRAWN BY: T.L.	APPROVED BY: J.A.P.
ADD "DRIVEWAY APPROACH" FORMAT CHANGE	01/26/17	CHECKED BY: A.S.	JOSE PASTRANA DIRECTOR OF ENGINEERING
ADD TOOLED JOINT AT 5 FT. SPACING	08/07/18	EFFECTIVE DATE: JANUARY 18, 2017	SCALE: N.T.S. SHEET NO: DWY-02

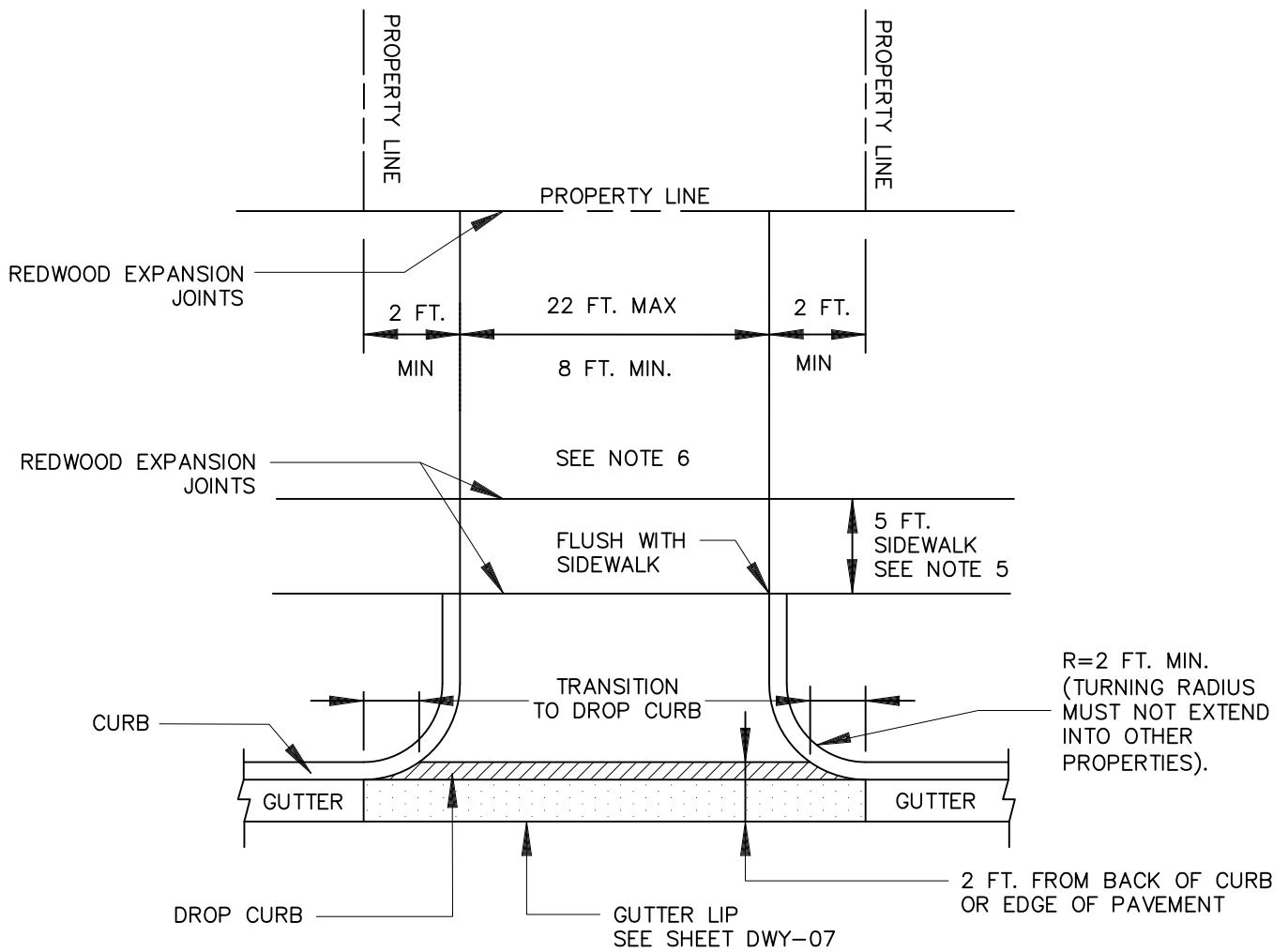


SECTION A-A

NOTES:

1. WATER METERS NOT ALLOWED IN PAVING, TO BE RELOCATED AT OWNER'S EXPENSE.
2. THICKNESS: MIN. OF 6 IN. PAVEMENT ON A CITY RIGHT-OF-WAY AND MIN. OF 4 IN. PAVEMENT ON PRIVATE PROPERTY.
3. REINFORCEMENT: #4 BARS WITH 12 IN. X 12 IN. CENTERS OR ENGINEERED DESIGN FOR H20+ WHEEL LOADING.
4. EXPANSION JOINTS: CONCRETE DRIVE SHALL BE PROVIDED WITH AN EXPANSION AT THE PROPERTY LINE AT 10 FT. INTERVALS AND ADJACENT TO EXISTING CONCRETE EXCEPT AT THE TIE-IN TO THE STREET.
5. CULVERT: ASTM C-76 CLASS IV RCP.
6. SIDEWALK: MAX. 2% CROSS SLOPE AND 5% LONGITUDINAL SLOPE. SEE SIDEWALK DETAILS.
7. DRIVEWAY APPROACH: 5% MAX. SLOPE, 2% MIN. SLOPE.
8. WITHIN PRIVATE PROPERTY THE MINIMUM DRIVEWAY REQUIREMENTS TO BE A MINIMUM 4 IN. THICK CONCRETE WITH #3 BARS AT MAX 24 IN. SPACING CENTER TO CENTER. AT EXPANSION JOINTS THERE SHALL BE A MINIMUM #3 DOWEL AT MAX 24 IN. SPACING.
9. PROVIDE TOOLED JOINT AT 5 FT. SPACING (SHEET DWY-02).

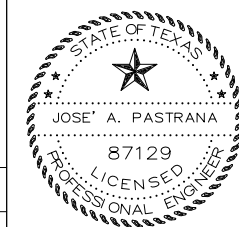
		<p align="center">CITY OF BAYTOWN DEPARTMENT OF ENGINEERING</p>	
		<p align="center">COMMERCIAL DRIVEWAY APPROACH (OPEN DITCH)</p>	
REVISION	DATE	DRAWN BY: T.L.	APPROVED BY: J.A.P.
ADD "DRIVEWAY APPROACH" FORMAT CHANGE	01/26/17	CHECKED BY: A.S.	JOSE PASTRANA DIRECTOR OF ENGINEERING
ADD NOTE 8 AND 9	08/07/18	EFFECTIVE DATE: JANUARY 18, 2017	SCALE: N.T.S.
		SCALE: N.T.S.	SHEET NO: DWY-03



NOTES:

1. WATER METERS NOT ALLOWED IN PAVING, TO BE RELOCATED AT OWNER'S EXPENSE.
2. THICKNESS: MIN. OF 5 IN. PAVEMENT ON A CITY RIGHT-OF-WAY AND MIN. OF 4 IN. PAVEMENT ON PRIVATE PROPERTY.
3. REINFORCEMENT: #4 BARS WITH 24 IN. X 24 IN. CENTERS.
4. EXPANSION JOINTS: CONCRETE DRIVE SHALL BE PROVIDED WITH AN EXPANSION AT THE PROPERTY LINE AND AT 10 FT. INTERVALS AND ADJACENT TO EXISTING CONCRETE EXCEPT AT THE TIE-IN TO THE STREET.
5. SIDEWALK: MAX. 2% CROSS SLOPE AND 5% LONGITUDINAL SLOPE.
6. DRIVEWAY APPROACH: 5% MAX. SLOPE, 2% MIN. SLOPE.

SEAL:



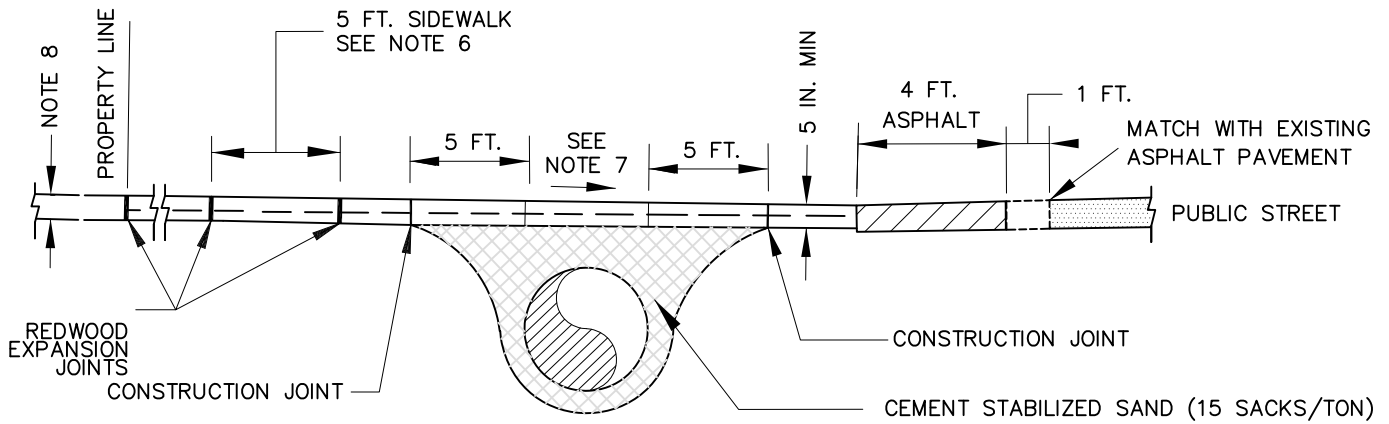
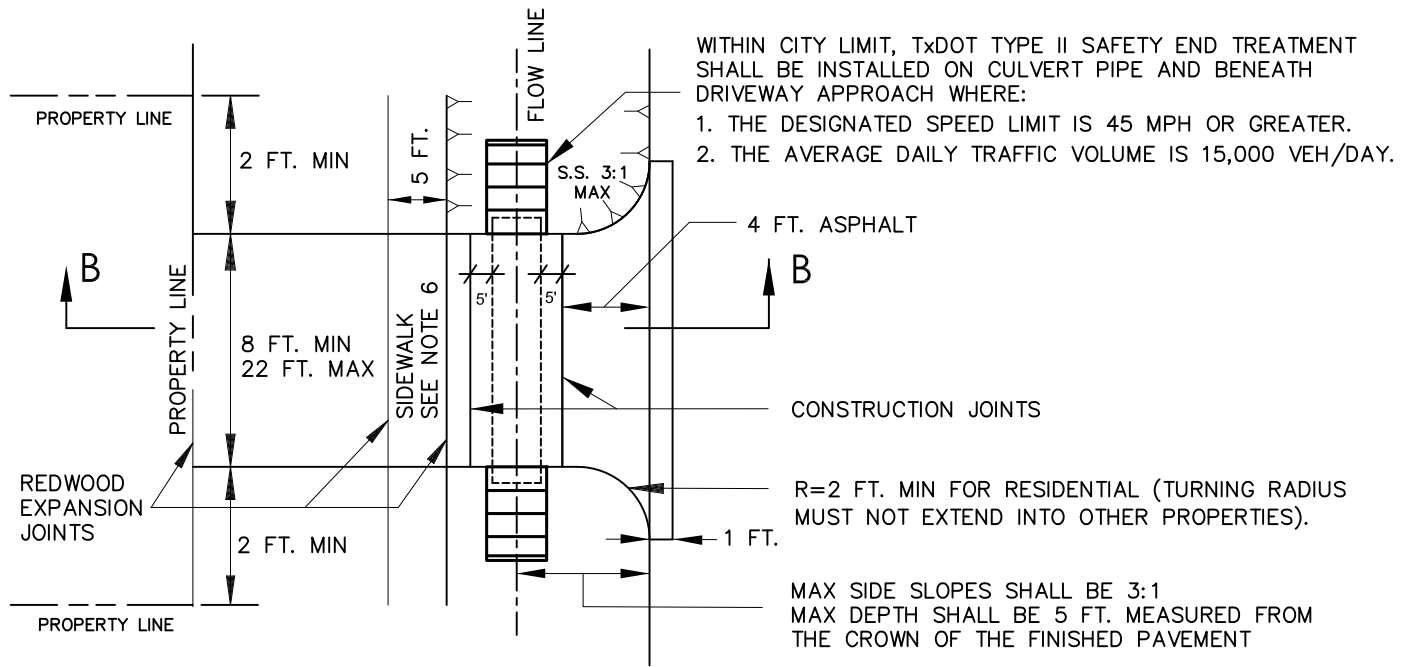
CITY OF BAYTOWN
DEPARTMENT OF ENGINEERING

RESIDENTIAL DRIVEWAY APPROACH

REVISION	DATE
ADD "DRIVEWAY APPROACH" FORMAT CHANGE	01/26/17

EFFECTIVE DATE: JANUARY 18, 2017

DRAWN BY: T.L.	APPROVED BY: J.A.P.
CHECKED BY: A.S.	JOSE PASTRANA DIRECTOR OF ENGINEERING
SCALE: N.T.S.	SHEET NO: DWY-04

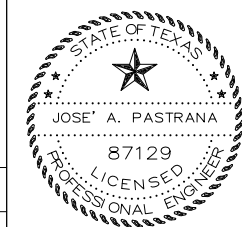


SECTION B-B

NOTES:

1. WATER METERS NOT ALLOWED IN PAVING, TO BE RELOCATED AT OWNER'S EXPENSE.
2. THICKNESS: MIN. OF 5 IN. PAVEMENT ON A CITY RIGHT-OF-WAY AND MIN. OF 4 IN. PAVEMENT ON PRIVATE PROPERTY.
3. REINFORCEMENT: #4 BARS WITH 24 IN. X 24 IN. CENTERS.
4. EXPANSION JOINTS: CONCRETE DRIVE SHALL BE PROVIDED WITH AN EXPANSION AT THE PROPERTY LINE AND AT 10 FT. INTERVALS AND ADJACENT TO EXISTING CONCRETE EXCEPT AT THE TIE-IN TO THE STREET.
5. CULVERT: ASTM C-76 CLASS III RCP OR CORRUGATED PLASTIC PIPE AASHTO M294 WITH MIN. PIPE STIFFNESS OF 46, MAX. DIAMETER OF 30 IN., AND HAVING GASKETED PIPE JOINTS. CONTACT COB PUBLIC WORK AT 281-420-5300 TO SET CULVERT AT OWNER'S EXPENSE.
6. SIDEWALK: MAX. 2% CROSS SLOPE AND 5% LONGITUDINAL SLOPE.
7. DRIVEWAY APPROACH: 5% MAX. SLOPE, 2% MIN. SLOPE
8. WITHIN PRIVATE PROPERTY THE MINIMUM DRIVEWAY REQUIREMENTS TO BE A MINIMUM 4 IN. THICK CONCRETE WITH #3 BARS AT MAX 24 IN. SPACING CENTER TO CENTER. AT EXPANSION JOINTS THERE SHALL BE A MINIMUM #3 DOWEL AT MAX 24 IN. SPACING.

SEAL:



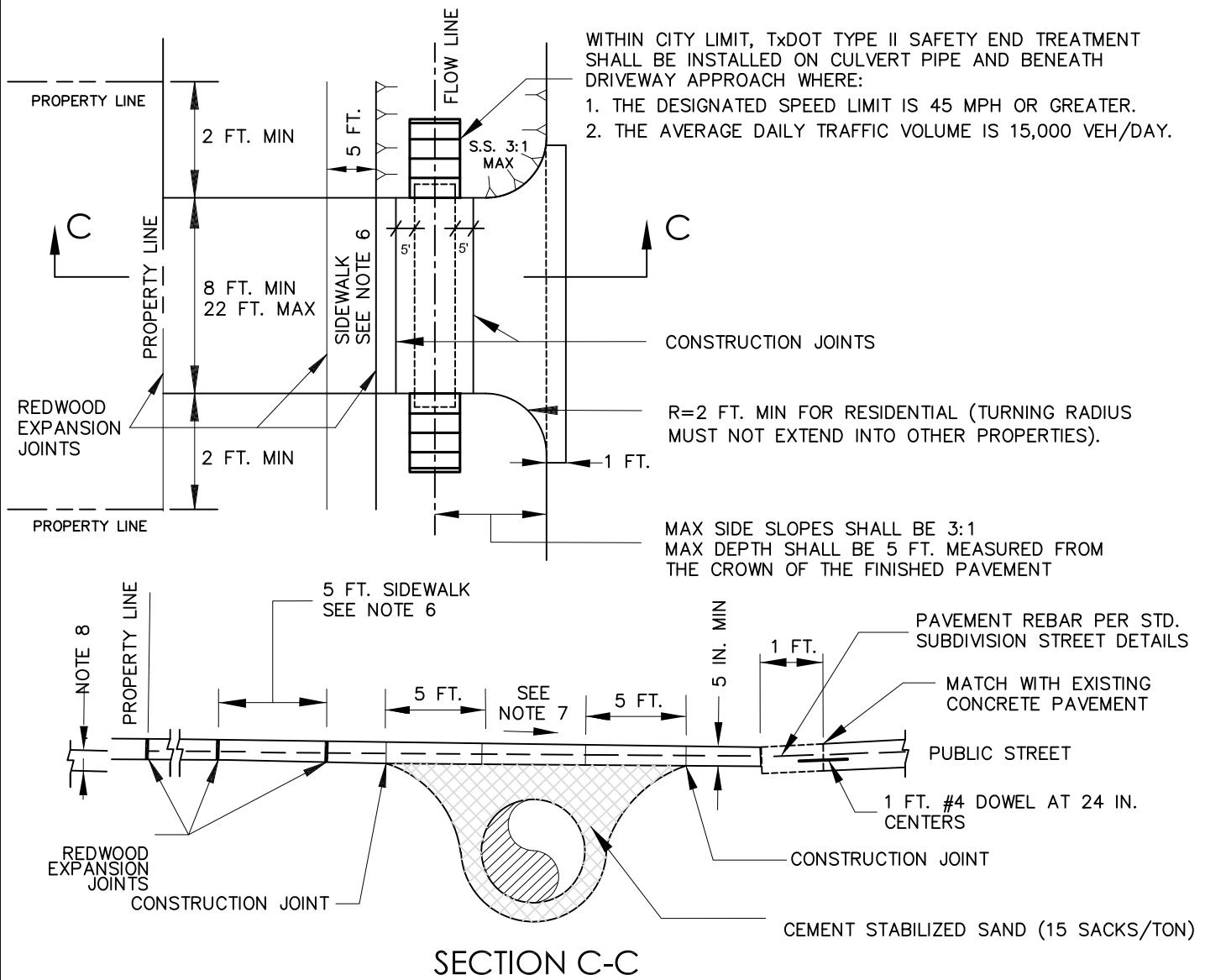
**CITY OF BAYTOWN
DEPARTMENT OF ENGINEERING**

RESIDENTIAL DRIVEWAY APPROACH
CONCRETE APPROACH TO ASPHALT ROAD (OPEN DITCH)

REVISION	DATE
ADD "DRIVEWAY APPROACH" FORMAT CHANGE	01/26/17
ADD NOTE 8 PRIVATE PROPERTY MINIMUM DRIVEWAY REQUIREMENTS	08/14/17

EFFECTIVE DATE: JANUARY 18, 2017

DRAWN BY: T.L.	APPROVED BY: J.A.P.
CHECKED BY: A.S.	JOSE PASTRANA DIRECTOR OF ENGINEERING
SCALE: N.T.S.	SHEET NO: DWY-05



NOTES:

1. WATER METERS NOT ALLOWED IN PAVING, TO BE RELOCATED AT OWNER'S EXPENSE.
2. THICKNESS: MIN. OF 5 IN. PAVEMENT ON A CITY RIGHT-OF-WAY AND MIN. OF 4 IN. PAVEMENT ON PRIVATE PROPERTY.
3. REINFORCEMENT: #4 BARS WITH 24 IN. X 24 IN. CENTERS.
4. EXPANSION JOINTS: CONCRETE DRIVE SHALL BE PROVIDED WITH AN EXPANSION AT THE PROPERTY LINE AND AT 10 FT. INTERVALS AND ADJACENT TO EXISTING CONCRETE EXCEPT AT THE TIE-IN TO THE STREET.
5. CULVERT: ASTM C-76 CLASS III RCP OR CORRUGATED PLASTIC PIPE AASHTO M294 WITH MIN. PIPE STIFFNESS OF 46, MAX. DIAMETER OF 30 IN., AND HAVING GASKETED PIPE JOINTS. CONTACT COB PUBLIC WORK AT 281-420-5300 TO SET CULVERT AT OWNER'S EXPENSE.
6. SIDEWALK: MAX. 2% CROSS SLOPE AND 5% LONGITUDINAL SLOPE.
7. DRIVEWAY APPROACH: 5% MAX. SLOPE, 2% MIN. SLOPE.
8. WITHIN PRIVATE PROPERTY THE MINIMUM DRIVEWAY REQUIREMENTS TO BE A MINIMUM 4 IN. THICK CONCRETE WITH #3 BARS AT MAX 24 IN. SPACING CENTER TO CENTER. AT EXPANSION JOINTS THERE SHALL BE A MINIMUM #3 DOWEL AT MAX 24 IN. SPACING.

SEAL:



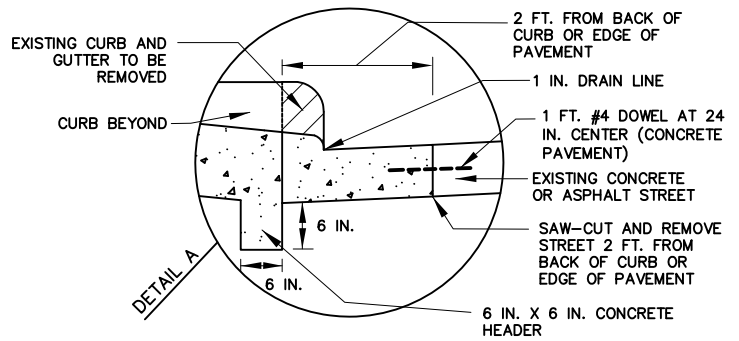
CITY OF BAYTOWN
DEPARTMENT OF ENGINEERING

RESIDENTIAL DRIVEWAY APPROACH
CONCRETE APPROACH TO CONCRETE ROAD (OPEN DITCH)

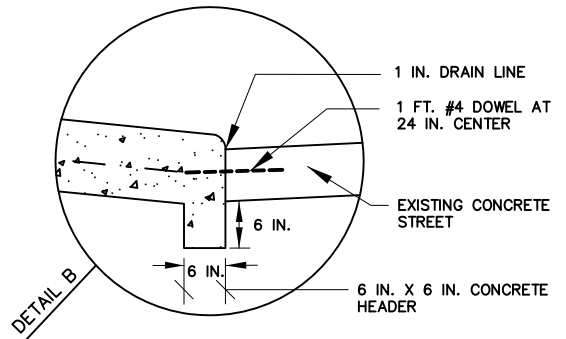
REVISION	DATE
ADD "DRIVEWAY APPROACH" FORMAT CHANGE	01/26/17
ADD NOTE 8 PRIVATE PROPERTY MINIMUM DRIVEWAY REQUIREMENTS	08/14/17

EFFECTIVE DATE: JANUARY 18, 2017

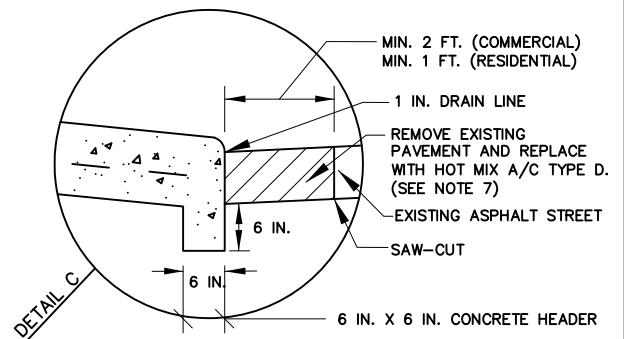
DRAWN BY: T.L.	APPROVED BY: J.A.P.
CHECKED BY: A.S.	JOSE PASTRANA DIRECTOR OF ENGINEERING
SCALE: N.T.S.	SHEET NO: DWY-06



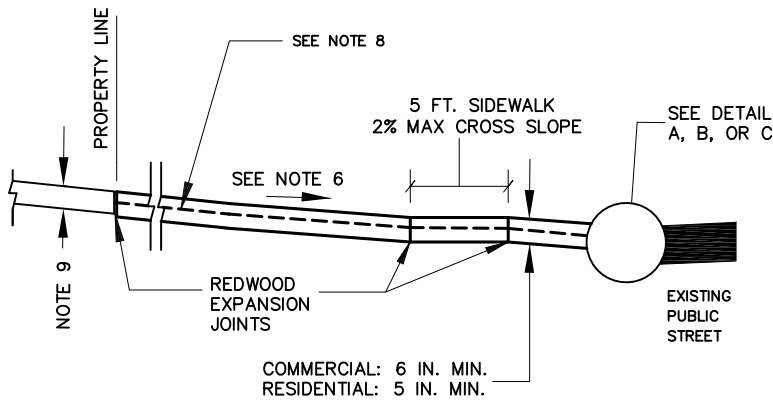
DETAIL A
CONCRETE TO CONCRETE WITH CURB



DETAIL B
CONCRETE TO CONCRETE W/O CURB



DETAIL C
CONCRETE TO ASPHALT W/O CURB



PROFILE

GENERAL NOTES:

1. PAVING HEADER IS NOT REQUIRED IF STREET HAS A CURB.
2. SAW CUTTING OF STREET IS NOT REQUIRED IF STREET DOES NOT HAVE CURB.
3. IF ASPHALT SHOULDER EXISTS, REMOVE SHOULDER & TIE CONCRETE DRIVEWAY INTO CONCRETE STREET & REPAIR SHOULDER AS NECESSARY. (EXAMPLE: GARTH RD).
4. FOR STANDING CURBS AND/OR DIRECTED BY ENGINEER, SAW CUT GUTTER LIP, REMOVE EXISTING CONCRETE, AND REPLACE AS PART OF NEW DRIVEWAY (SEE DETAIL A, B, OR C).
5. SIDEWALK: MAX 2% CROSS SLOPE AND 5% LONGITUDINAL SLOPE.
6. DRIVEWAY APPROACH: MAX. 5% SLOPE, MIN. 2% SLOPE.
7. REMOVAL NOT NECESSARY IF HAVE A CLEAN VERTICAL FACE OF ASPHALT PAVEMENT.
8. COMMERCIAL: #4 BARS WITH 12 IN. X 12 IN. CENTERS OR ENGINEERED DESIGN FOR H2O WHEEL LOADING.
8. RESIDENTIAL: #4 BARS WITH 24 IN. X 24 IN. CENTERS
9. WITHIN PRIVATE PROPERTY THE MINIMUM DRIVEWAY REQUIREMENTS TO BE A MINIMUM 4 IN. THICK CONCRETE WITH #3 BARS AT MAX 24 IN. SPACING CENTER TO CENTER. AT EXPANSION JOINTS THERE SHALL BE A MINIMUM #3 DOWEL AT MAX 24 IN. SPACING

SEAL:



CITY OF BAYTOWN
DEPARTMENT OF ENGINEERING

COMMERCIAL & RESIDENTIAL
DRIVEWAY APPROACH PROFILE

REVISION	DATE
ADD "DRIVEWAY APPROACH" FORMAT CHANGE	01/26/17
ADD NOTE 9 PRIVATE PROPERTY MINIMUM DRIVEWAY REQUIREMENTS	08/14/17

EFFECTIVE DATE: JANUARY 18, 2017

DRAWN BY: T.L.	APPROVED BY: J.A.P.
CHECKED BY: A.S.	JOSE PASTRANA DIRECTOR OF ENGINEERING
SCALE: N.T.S.	SHEET NO: DWY-07